

Director's Notes

I have been reading a book titled *"The Big Roads: The Untold Story of the Engineers, Visionaries, and Trailblazers Who Created the American Superhighways"* by Earl Swift. The book gives a fascinating insight into the development of our highway system.



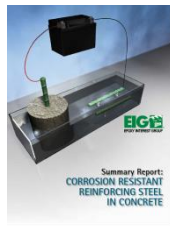
Initially, paved roads were developed to assist the growing sport of cycling, which did not wish to compete with the mud and waste from horses. The adoption of cars in the 1910s was rapid, with 3.37 million automobiles in use in 1916. This boom caused a crisis within the states.

In the 1910s, there were embittered arguments as to whether the country should build a national system of highways or a system of national highways. Federal aid for projects was discussed and criticized. Discussions occurred over bonding for roads. There were also discussions regarding the role of the federal government and whether roads should be local or interstate.

What is surprising about the book is that the same arguments used in the 1910s are being used in the 2010s as we look to develop a robust long-term transportation funding bill. For this reason, I recommend that you read this fascinating book.

New Report

EIG has published a new report titled **Summary Report: Corrosion Resistant Reinforcing Steel in Concrete**, which summarizes work from the University of Utah. It states: "The data suggest that even improperly handled and placed epoxy-coated reinforcing steel is the superior choice for designers looking to achieve 100-year life cycles."



Save the Date

The EIG Fall Business Meeting will be held on Sunday, November 2 at 3:30 pm at the Swissotel, Chicago. The meeting will include updates on activities in 2014, planned activities in 2015 and budgets. The meeting provides a forum for members to provide input into the activities of EIG.

Michigan DOT and Dowels

EIG was approached by Michigan DOT to assist with development of a future qualified products list for epoxy-coated dowels. MDOT wishes to require all dowels to be coated using ASTM A943 materials according to ASTM A1078. EIG understands that the specification will not require end coating. The change is believed to be required in 2015.

World of Concrete 2015

In 2015, EIG will have a booth at the World of Concrete. Please let EIG know if you would like to help with the exhibit.



PCI Bridge Meeting

EIG exhibited at the PCI Bridge Meeting in Washington DC. The meeting was attended by over 1,100 people, including a wide number of DOT staff and design engineers. Competitors to EIG were also present at the meeting, including Southern Atlantic Galvanizing, MMFX and several glass fiber reinforcing bar manufacturers.

National Concrete Consortium

EIG attended the National Concrete Consortium meeting in Omaha NE. This meeting had a session on the recently formed Dowel Bar Task Group, of which Dr. McDonald is a member. The aim of the group is to provide a specification for epoxy-coated and other dowel products. Issues such as corrosion resistance, robustness, structural and long-term durability are being discussed. Along with several EIG members, competitors from zinc-clad, fiberglass-clad and fiberglass dowel companies were present at the meeting.



ARTBA Report

The American Road & Transportation Builders Association (ARTBA) recently reported that state and local government awards for highway and bridge work have declined by nearly 14 percent in the first half of 2014, compared with the same period last year (see: <http://tinyurl.com/qjq99yw>).

The pipeline for bridge and highway work has slowed and the slowdown continued into the summer construction season, when state and local governments put out most of their awards. Half of U.S. states are awarding fewer bridge and highway contracts than they did a year ago. Contract activity in five other states was flat.

The construction slowdown is widespread, with 25 states awarding fewer highway and bridge contracts by June 2014 than they did a year ago, according to ARTBA. Awards were up in 21 states and Washington DC, and programs were fairly flat in five states, either up or down within five percent.



CRSI Certification

Straight Bar Lines and Fabrication: CRSI has initiated reviews of Epoxy Plant Quality Manuals according to the new ANSI Standards. During these reviews, several

errors and omissions have been noted in these standards and these will be put to the various committees at the CRSI Fall Business Meeting. Please let EIG know if you have any questions or wish corrections to be made to these documents.

Custom Lines: The CRSI Custom Standard is being balloted within the Standards Committee and any issues will need to be resolved in the Fall Business Meeting.

Bar Supports: As discussed in an email to EIG members, CRSI has been approached by one company to certify manufacture of epoxy-coated bar supports. As part of the assessment, CRSI is required to develop a business case for the program. If your organization is interested in certification of epoxy-coated bar supports please let Danielle Kleinhans know before the Fall Meeting.

Project Profiles

During the past two months, EIG has put the following project profiles on the EIG website:

- Craigflower Bridge, BC
- Dresbach Bridge, MN/WI
- Henley Bridge, TN
- South Park Bridge, WA
- Willapa Bridge, WA
- Yanbu Power Station Saudi

These projects can be found at www.epoxyinterestgroup.org. Please let EIG know if you have projects you would like featured.

Quote

The pessimist complains about the wind; the optimist expects it to change; the realist adjusts the sails.

William Arthur Ward

Date	Event
October 26 – 29	ACI, Washington DC
October 27 – 29	ASBI, Hartford, CT
November 2 – 6	CRSI, Chicago, IL
November 10 – 13	ASTM, New Orleans, LA
December 4 – 5	Accelerated Bridge Conference, Miami, FL
January 11 – 15	TRB Washington DC
February 3 – 6	World of Concrete, Las Vegas, NV
March 9 – 13	CRSI Technical Meeting, Tempe, AZ

