

Director's Notes

During the past several months, EIG members have been adjusting to changes in the CRSI Epoxy Certification program. The new program has required plants to provide individualized quality control manuals, which are then reviewed for compliance with the CRSI Standard. Corrective action methods have been specified and these are plant specific.



EIG has been heavily involved in the process, developing a Quality Manual template to assist the plants in conforming to the new standard. It is not meant to be adopted without change, but provides a starting point for the quality control manuals.

Plants should note that CRSI and WJE are unable to provide specific information about how to write the Quality Manual. It states in the CRSI document "Procedures for the Development and Maintenance of Manufacturing Certification Programs" that:

The CA (CRSI) shall not give advice or provide consultancy services to the applicant as to methods of dealing with matters which are barriers to the certification requested. This excludes issues specifically addressed in this document regarding disputes and reports.

The IA (WJE) shall not give advice or provide consulting services to the applicant as to methods of dealing with matters which are barriers to the certification requested.

Despite the restrictions on CRSI and WJE, EIG is able to assist with specific questions **on** how to respond to questions regarding quality manuals and inspections. Please let me know if you need help in this regard. Also, please let EIG know if you have specific recommendations for changing the program in the future.

IDOT Training

The IDOT training will occur on February 19, 2015 in Springfield, Illinois. Please make sure that your staff registers for this meeting.

Transportation Construction Coalition (TCC) Fly-In

The success of the epoxy-coated reinforcing steel industry is largely held by activities from the federal government in providing a robust transportation bill. On April 14-15, at the Hyatt Regency Washington, Washington, DC, the Transportation Construction Coalition will be meeting. Registration and other details will be available closer to the event date.

If you are unable to attend the meeting in Washington, please reach out to your federal representatives and let them know how transportation funding affects the economics of your company or community. If you want to find your representative, go to <https://www.opencongress.org/people/zipcodelookup> and enter your details for a list of both the senators and representatives. Generally, the representatives have meetings in your local area when the senate is at recess and you can meet with their staff to describe your needs. Otherwise, send them an email specifically outlining your concerns.



ASTM

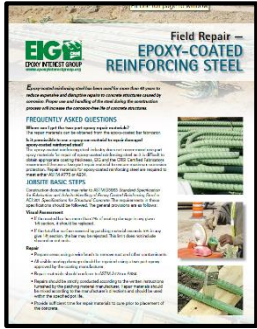
A main committee ballot for ASTM A775 has been issued. Additional subcommittee ballots for ASTM A775 have also been balloted that propose changes to inspection requirements and reduce the amount of CD testing from 1 per 8 hours to 1 per 24 hours.

Work is also being conducted on adding a more realistic damage test for epoxy coatings. This test involves scratching bars with a steel scribe, loaded to a specific load. All four powder members of EIG are involved in this process and it is not expected that there will be additional plant tests.

Article for Ironworker Magazine

EIG recently completed an article for the Ironworker Magazine titled "Use And Handling Of Epoxy-Coated Reinforcing Steel." It is expected that this will be published in the Rebar edition of the Ironworker Magazine which is printed in June/July of each year.

Please let EIG know if there are other places EIG should be placing articles.



New Publication from EIG

Based upon feedback from several shows, a two-page document on field handling and field repair of epoxy-coated reinforcing steel has been developed. This document will be used at the World of Concrete and other trade shows. Please let EIG know if you would like copies sent to you.

of bridges with concrete that could last 30-40 years at best, or half as many using composites that could last up to 100 years.

Anti-Corrosion Times

The latest edition of Anti-Corrosion Times will be sent soon with articles on the cost effectiveness of reinforcing steel, the Dresbach Bridge in Minnesota, the I-65 CRCP project in Indiana and the Miami Science Museum in Florida. Additional information is also provided on the field repair of epoxy-coated reinforcing steel.



UNM 51st Paving and Transportation Conference, Albuquerque, NM

EIG presented a talk titled "Options for corrosion protection of concrete structures" to approximately 80 people at the University of New Mexico Transportation Conference. The topic included improvements in concrete to reduce cracking and a review of sustainability issues for reinforcing steel. Please let EIG know if you would like a copy of the presentation.

Please let EIG know if you have articles or projects you wish to have featured.



TRB Washington, DC

At the recent TRB meeting, EIG was active in the Corrosion and Bridge committees. The meeting was attended by 12,000 people from all over the world. EIG was able to represent the National Concrete Bridge Council in several meetings.

Quote

Golden bridge, silver bridge or diamond bridge; it doesn't matter! As long as the bridge takes you across the other side, it is a good bridge!

Mehmet Murat Ildan

In the exhibit hall, Steel Dynamics from Roanoke, VA, presented a stainless-clad reinforcing steel bar and there was a presentation on this product in the Corrosion committee meeting.

Date	Event
February 3 – 6	World of Concrete, Las Vegas, NV
February 19	IDOT Training, Springfield, IL
March 9 – 13	CRSI Technical Meeting, Tempe, AZ
April 12 – 16	ACI, Kansas City, MO
April 14 – 15	Legislative Fly-In, Washington, DC
April 19 – 23	AASHTO Subcommittee on Bridges, Saratoga Springs, NY
April 30 – May 2	PCI Committee Days, Chicago, IL
May 4 - 6	CRSI Spring Meeting, San Antonio, TX
May 18 – 21	NTPEP, Scottsdale, AZ
May 18 – 21	ASTM, Anaheim, CA



Composites World

In a recent article presented in Composites World, the civil infrastructure market was discussed. *"In many locales, the useful life of corrosion-prone steel-rebar-reinforced concrete is limited to 25 years, rather than the 75 to 100 years once promised by its advocates. Therefore, the lifecycle cost advantages, not to mention the safety benefits, of using composite rebar continue to overcome resistance among change-averse municipalities...Faced with limited annual budgets, state and local transportation executives have the choice to replace a certain number*